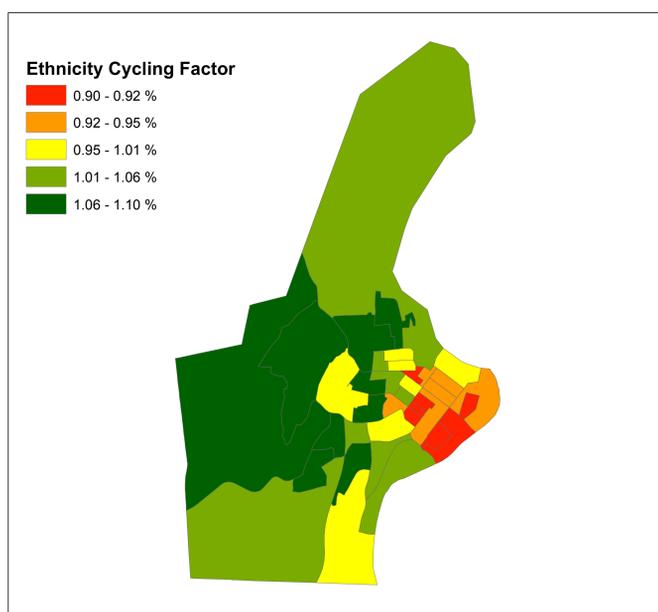
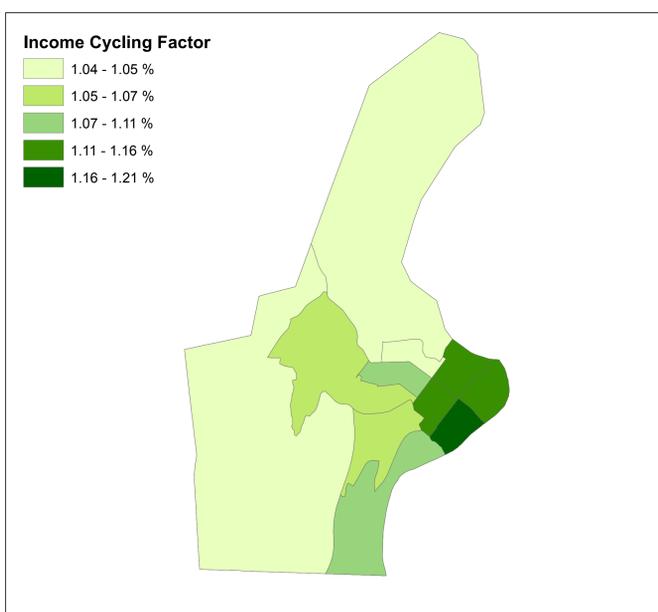


Overall, Americans make 1% of all of their trips by bicycle. Some populations, however, have higher or lower bicycle trip rates than average. This map shows key demographic factors, and how they may impact bicycle trip rates in Holyoke.

The top six panels of the map show key demographic factors by census geography. Populations who make the highest percent of their trips by bicycle include 5-15 year old (3.1% of trips by bicycle), households that do not own a car (2.5%), males (1.6%), and households in the lowest income quartile (1.3%). Populations who make the lowest percent of trips by bicycle include females (0.5% of trips by bicycle), people who are 65 years old or older (0.6%), and 25-39 year olds (0.7%). Bicycling rates vary by ethnicity but the range is relatively small (from 1.1% for whites to .9% for Asians and Hispanics).

The bottom five panels show demographic-based “cycling factors.” Higher numbers (shades of green) indicate that we would expect a higher than average bicycle trip rate from that area based on demographic factors (more than 1%). Lower numbers (shades of orange and red) have the opposite meaning—we expect lower than average bicycle trip rates from that area. The “cycling factors” show the combined impact of both variation in population characteristics across Holyoke, and variation in bicycling rates by population.

This map is intended to help Holyoke prioritize bicycle infrastructure investments. Holyoke could choose to prioritize bicycle improvements in areas that currently have high bike trip rates. And/or Holyoke could choose to prioritize areas where bike trip share may be low, but where the populations might respond favorably to more safe, or convenient infrastructure. For example, when cities improve the safety of bicycle infrastructure, it generally has more impact on how often females bicycle than males.



2015 Bicycle Plan Demographics & Predicted Bicycle Trip Rates

HOLYOKE, MA